

RIDGEFIELD HIGH SCHOOL WATER MAIN EXTENSION

RIDGEFIELD, CONNECTICUT CONTRACT DRAWING

GENERAL NOTES:

1. BASE MAPPING WAS PREPARED FROM FIELD WORK CONDUCTED BY SNYDER CIVIL ENGINEERING, LLC IN SEPTEMBER OF 2021 AND SUPPLEMENTED WITH INFORMATION FROM THE FOLLOWING MAPS:
 - A. USGS HIGH-RESOLUTION ORTHO IMAGERY
 - B. "WATER MAIN REPLACEMENT PROJECTS 2020 - BID GROUP 24 CRAIGMOOR ROAD WATER MAIN REPLACEMENT, RIDGEFIELD, CONNECTICUT, CONTRACT DRAWING, MARCH 2020, REVISED 04-17-2020, PREPARED FOR: AQUARIAN WATER COMPANY OF CT, PREPARED BY: SNYDER CIVIL ENGINEERING, LLC"
 - C. "RURAL WATER COMPANY CRAIGMOOR ROAD WATER SYSTEM SITE PLAN, RIDGEFIELD, CT, DATE: 3/21/2002, SCALE: AS NOTED, SHEET NO. 1 OF 1."
 - D. "PROPERTY / BOUNDARY SURVEY, CRAIGMOOR ROAD RIDGEFIELD, CONNECTICUT" DATED FEBRUARY 4, 2015, PREPARED BY JACKOWIAK LAND SURVEYING.
 - E. "TOWN OF RIDGEFIELD, CONNECTICUT, NORTH STREET WATER MAIN, CONTRACT 07-1, DPH PROJECT NUMBER 2007 1180013a, MARCH 2008, METCALF & EDDY / AECOM"
 - F. TOWN OF RIDGEFIELD, CONNECTICUT, MIDDLE SCHOOL PUMP STATION AND FORCE MAIN, STATE PROJECT NO. 118-044N, NORTH SALEM ROAD (RTE. 116), 14 MARCH 2001, REVISIONS 1/24/03, METCALF & EDDY"
2. APPROXIMATE PROPERTY LIMITS WERE OBTAINED FROM THE TOWN OF RIDGEFIELD, CT. ASSESSMENT PARCEL MAPS NO. 120 (ACCESSED ONLINE IN AUGUST OF 2021)
3. REFERENCE FOR THE LOCATION OF SUBSURFACE OR UNDERGROUND UTILITIES AND STRUCTURES SHOWN ON THE DRAWINGS IS BASED ON INFORMATION PROVIDED BY THE FOLLOWING:
 - ELECTRIC - FIELD LOCATION AND MAPPING PROVIDED BY EVERSOURCE ENERGY
 - STORM DRAINAGE - FIELD LOCATION AND MAPPING PROVIDED BY TOWN OF RIDGEFIELD
 - WATER - FIELD LOCATION AND MAPPING PROVIDED BY AWC
 - SANITARY SEWER - FIELD LOCATION AND MAPPING PROVIDED BY TOWN OF RIDGEFIELD
 - COMMUNICATIONS - FIELD LOCATION AND MAPPING PROVIDED BY FRONTIER COMM.
 - GAS - N/A
4. THE LOCATION OF ALL SUBSURFACE OR UNDERGROUND UTILITIES AND STRUCTURES INCLUDING BUT NOT LIMITED TO PIPES, CABLES AND CONDUITS ARE NOT GUARANTEED TO BE EXACT, NOR IS IT GUARANTEED THAT ALL UNDERGROUND OR SUBSURFACE ITEMS ARE SHOWN.
5. THE EXACT TYPE, SIZE, LOCATION AND DEPTH OF EXISTING WATER MAINS IS UNKNOWN. THE EXISTING WATER MAINS ON THIS PLAN ARE SHOWN FOR SCHEMATIC PURPOSES ONLY BASED ON THE BEST AVAILABLE INFORMATION OBTAINED BY FIELD LOCATION AND MAPPING AND INFORMATION PROVIDED BY AQUARIAN WATER COMPANY.
6. MINIMUM WATER MAIN SANITARY INSTALLATION CLEARANCES ARE LISTED AS FOLLOWS:

FOR WATER MAINS INSTALLED BELOW SANITARY SEWERS OR STORM DRAINS - 18" VERTICAL CLEARANCE

FOR WATER MAINS INSTALLED ABOVE SANITARY SEWERS OR STORM DRAINS - 12" VERTICAL CLEARANCE

IF MINIMUM INSTALLATION CLEARANCES CANNOT BE MET, THE CONTRACTOR SHALL

 - NOTIFY THE ENGINEER
 - PROVIDE A WATER TIGHT SEAL AT JOINTS WITHIN 10' OF THE CROSSING USING A PIPE JOINT REPAIR CLAMP OR MECHANICAL JOINT
 - INSTALL A 2" RIGID INSULATION PROTECTIVE BARRIER BETWEEN THE WATER MAIN AND SEWER OR STORM DRAIN
7. AQUARIAN WATER COMPANY RESERVES THE RIGHT TO ADD, SUBTRACT OR RELOCATE TEST PITS AS DIRECTED BY THE PROJECT MANAGER.
8. ABANDON ALL HYDRANTS, GATE BOXES, AND OTHER APPURTENANCES A MINIMUM OF 2' BELOW GRADE.
9. ALL-SURFACE RESTORATION INCLUDING PAVEMENT, CURBING, LANDSCAPING, PLANTING AND TREE REPLACEMENT SHALL BE COORDINATED WITH THE PROPERTY OWNERS AND THE TOWN OF RIDGEFIELD.
10. ALL VALVES SHALL BE OPEN RIGHT (OTR).

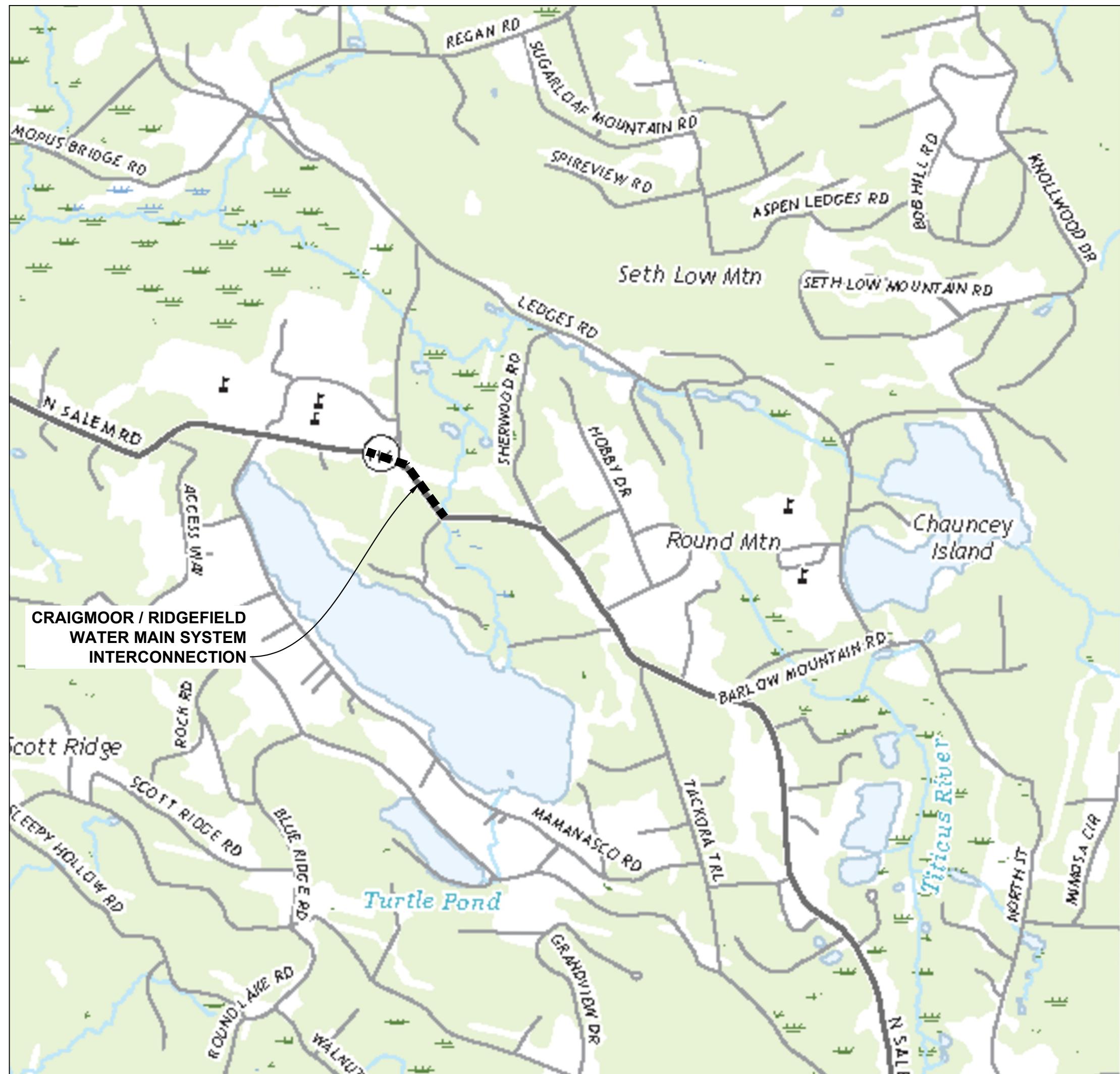
819 CONSTRUCTION NOTES:

1. REMOVAL OF PAVEMENT MARKINGS ALONG STATE ROADWAYS SHALL BE COMPLETED BY A NON-DESTRUCTIVE METHOD IN COMPLIANCE WITH THE STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION FORM 819 SECTION 12.11, AS REVISED.
2. NEW PAVEMENT MARKINGS SHALL BE PAINTED WITH EPOXY RESIN PAINT IN COMPLIANCE WITH THE STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION FORM 819 SECTION 12.10 AS REVISED.
3. NEW SIGN MATERIAL AND SHEETING SHALL BE MADE OF REFLECTIVE MATERIAL IN COMPLIANCE WITH THE STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION FORM 819 SECTION 12.08 AS REVISED. TYPE 1 REFLECTIVE SHEETING SHALL BE USED FOR SIGNS WITH WHITE BACKGROUND, TYPE 3 REFLECTIVE SHEETING SHALL BE USED FOR SIGNS WITH COLORED BACKGROUND EXCEPT FOR SIGNS WITH RED BACKGROUND THAT SHALL BE TYPE 8 OR 9 REFLECTIVE SHEETING.
4. ALL SIGNS AND PAVEMENT MARKINGS INSTALLED ALONG THE STATE ROAD MUST CONFORM TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES," THE LATEST STATE OF CONNECTICUT CATALOG OF SIGNS AND STANDARDS, AS REVISED.
5. ANY DAMAGE TO THE EXISTING CURB, SIDEWALK OR ANY OTHER HIGHWAY APPURTENANCES DURING THE DEVELOPMENT OF THE PERMITTED SITE WILL BE REPLACED BY THE CONTRACTOR, AS DIRECTED BY THE DISTRICT PERMIT SECTION, AT NO COST TO THE STATE.
6. ALL WORK WITHIN THE STATE RIGHT OF WAY WILL COMPLY WITH THE STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION FORM 819 WITH THE LATEST SPECIAL PROVISIONS AND TYPICAL STATE STANDARD DETAILS.

PREPARED FOR:



TOWN OF RIDGEFIELD
400 MAIN STREET
RIDGEFIELD, CT 06877



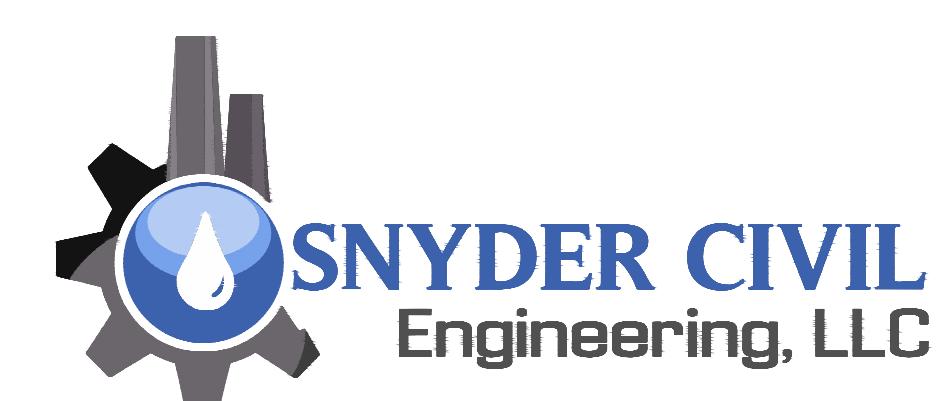
LOCATION PLAN

SCALE: 1" = 1000'

0 1000' 2000'

SHEET DIRECTORY	
SHEET NO.	SHEET CONTENTS
1	NORTH SALEM ROAD PLAN STA. 0+00 TO STA. 9+53
2	WATER MAIN, PAVEMENT AND EROSION AND SEDIMENTATION CONTROL DETAILS
3	WORK ZONES LAYOUT, TRAFFIC PLAN STA. 0+00 TO STA. 9+53
4	MAINTENANCE AND PROTECTION OF TRAFFIC PLAN NOTES AND DETAILS
5	CONSTRUCTION SIGNS, BARRICADES, CONES AND DRUMS DETAILS
6	CT DOT PAVING DETAILS

PREPARED BY:



SNYDER CIVIL ENGINEERING, LLC
150 MARLBOROUGH STREET
PORTLAND, CT 06480

CATCH BASIN	ROCK/BOULDER	STONE WALL	VALVE
STREET SIGN	SURVEY CONTROL POINT	COMM. LINE	TEE
MAILBOX	SPOT ELEVATION	ELEC. LINE	HYDRANT
SHRUB/BUSH	IRON PIN/PIPE	SANITARY SEWER MAIN	CURB BOX
VALVE	CONIFEROUS TREE	WATER MAIN	WATER METER
HYDRANT	DECIDUOUS TREE	GAS MAIN	REDUCER
CURB VALVE	TREE STUMP	TRAFFIC CTRL LINE	COUPLING OR SLEEVE
WATER METER	UTILITY POLE W/ GUY WIRE	TRAILER/BRUSHLINE	ENDCAP
LIGHT POST	GARDEN	PROPERTY LINE	INSERTION VALVE
MANHOLE	TRAFFIC LOOP	STORM DRAIN PIPE	BLOW-OFF OR AIR VENT
FENCE	CROSS WALK	RETAINING WALL	WATER MAIN
		SHRUB ROW	SERVICE LINE

NOTE:
 • EXISTING FEATURES ARE DRAWN IN THIN LIGHT GREY, PROPOSED FEATURES ARE DRAWN BOLD AND LIGHT BLUE.
 • ALL DRAIN PIPE AND SANITARY SEWER ELEVATIONS SHOWN WITH MULTILEADERS ARE INVERT ELEVATIONS UNLESS OTHERWISE NOTED

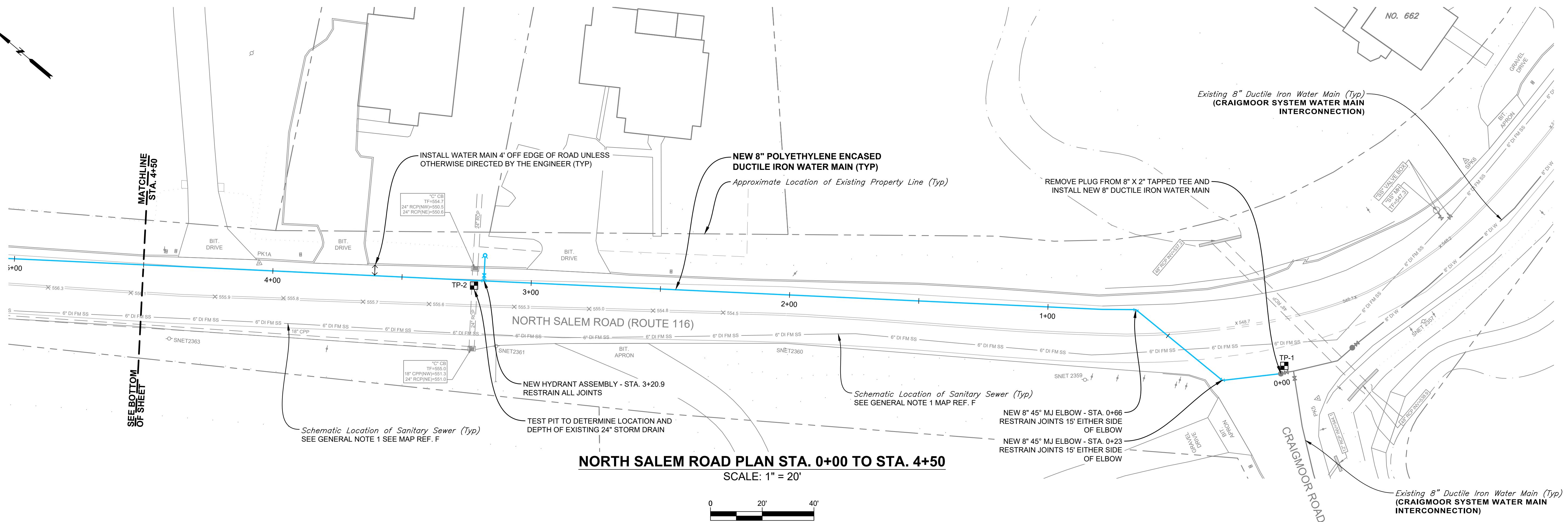
CONTRACT
DRAWING
NORTH SALEM ROAD
PLAN STA. 0+00 TO STA. 9+53

SCALE: 1" = 20'

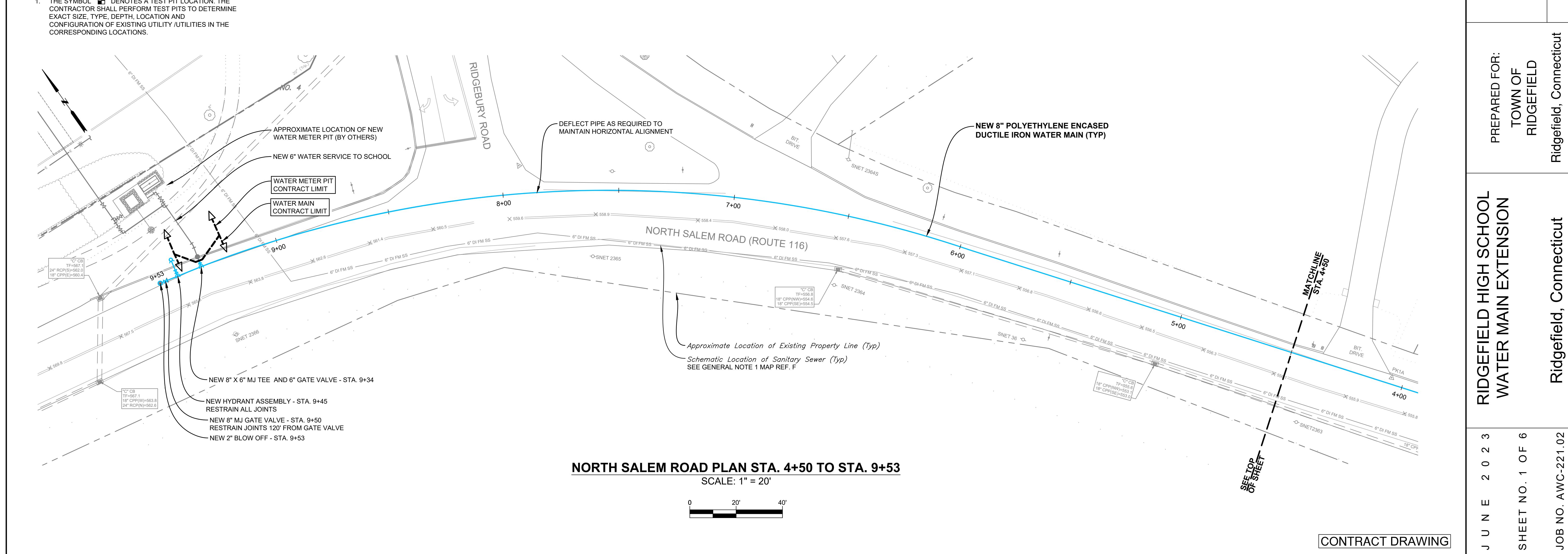
DRAWN KPP

REVIEWED

REvised



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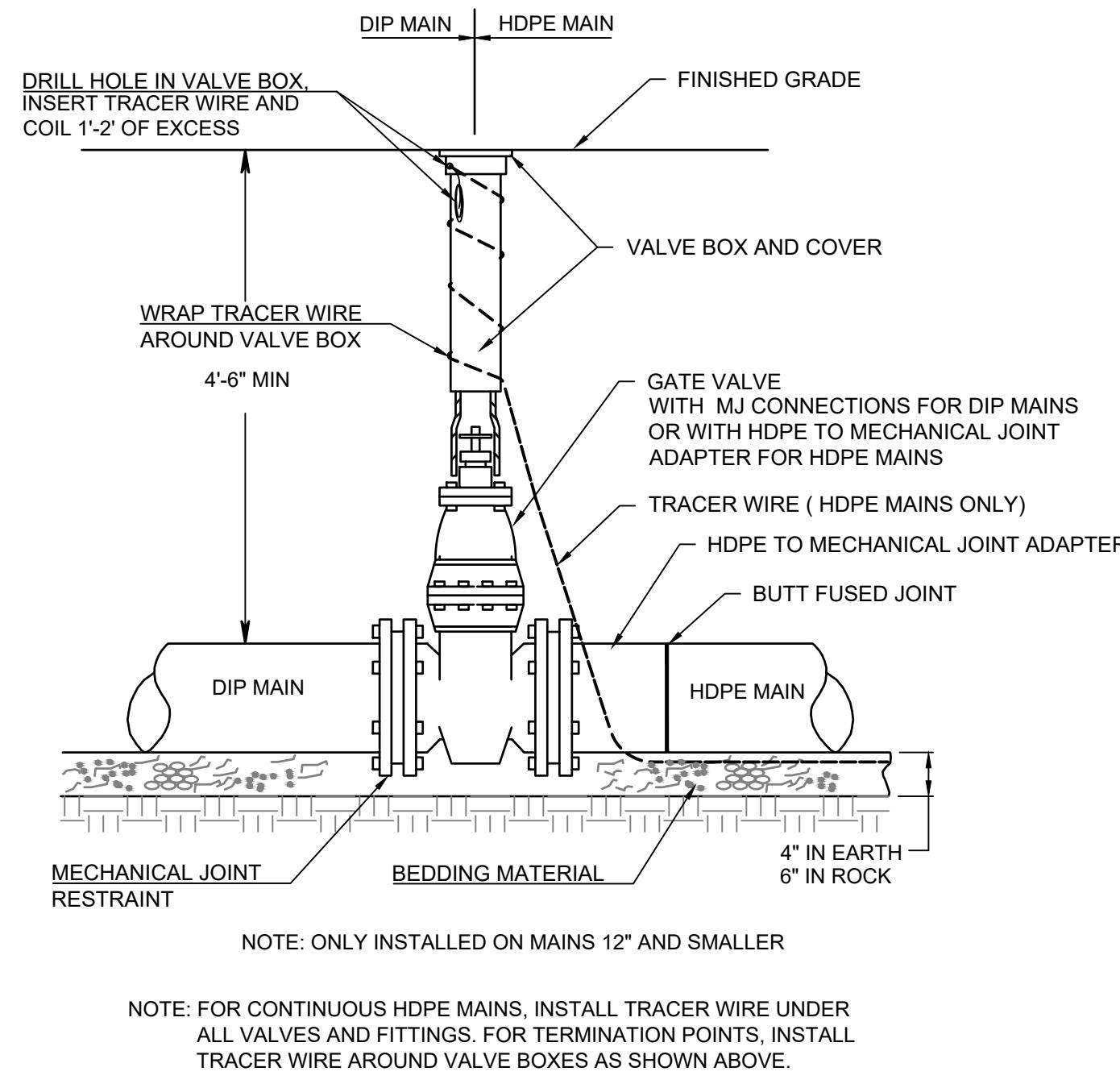
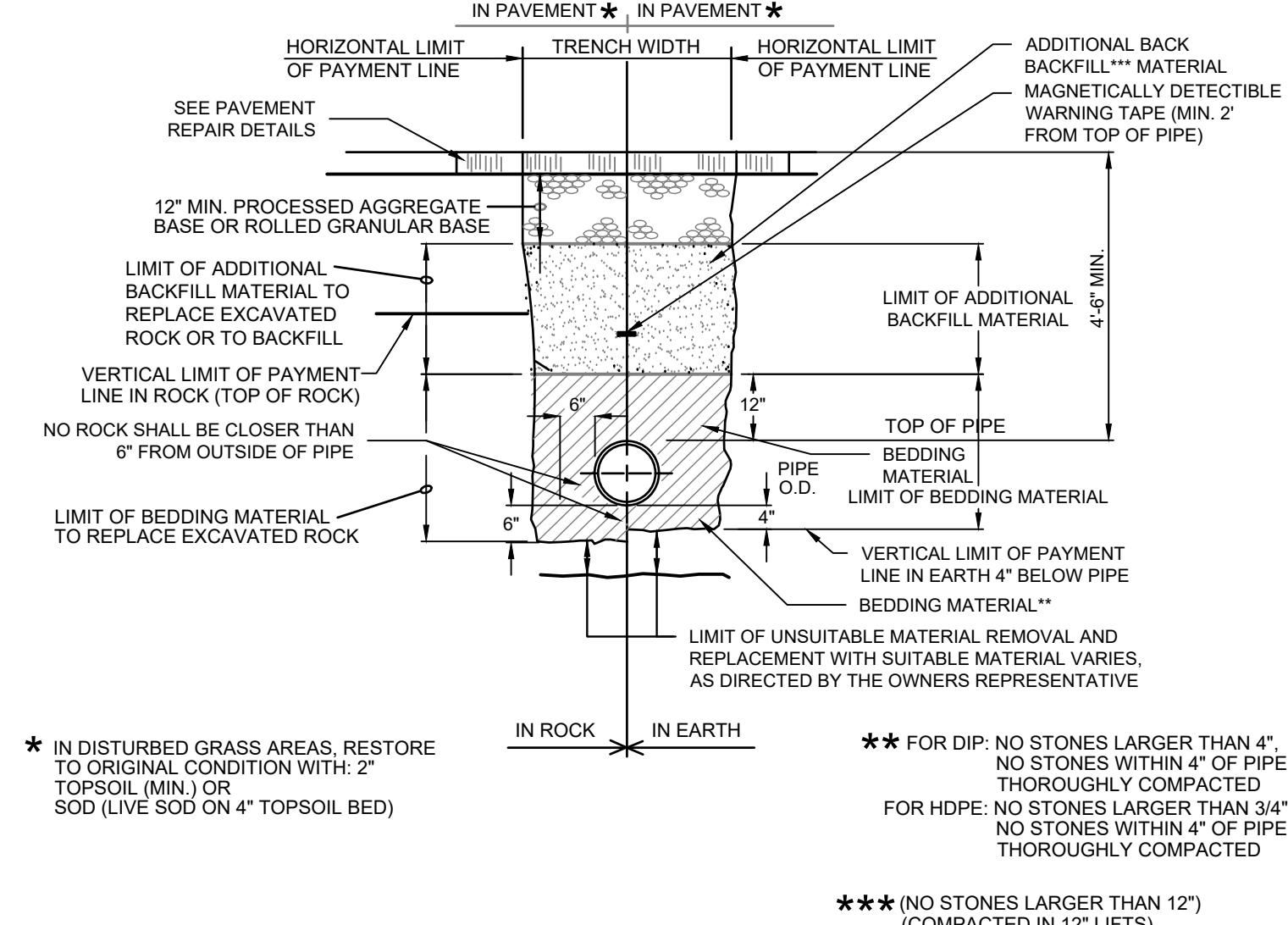
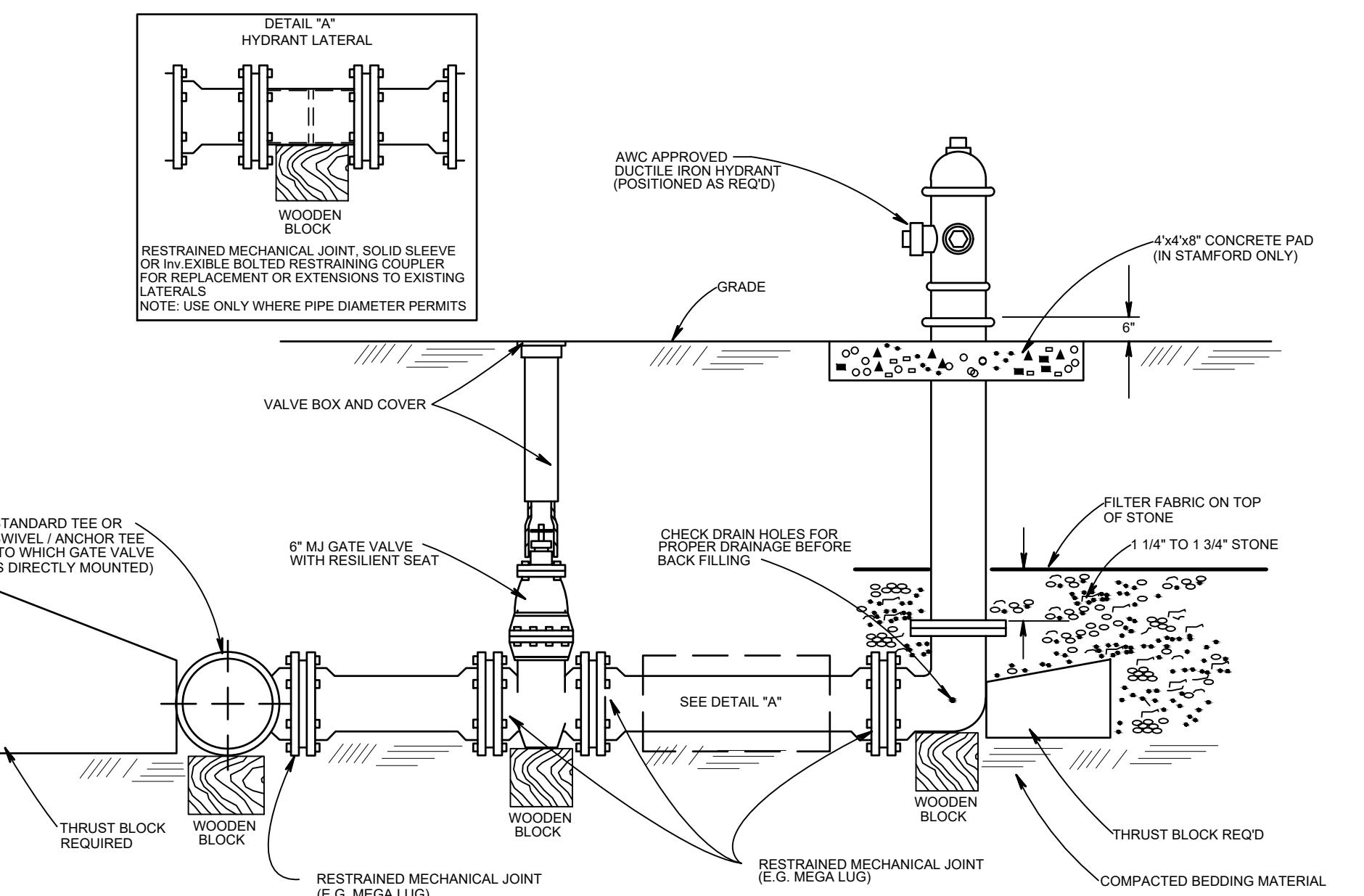
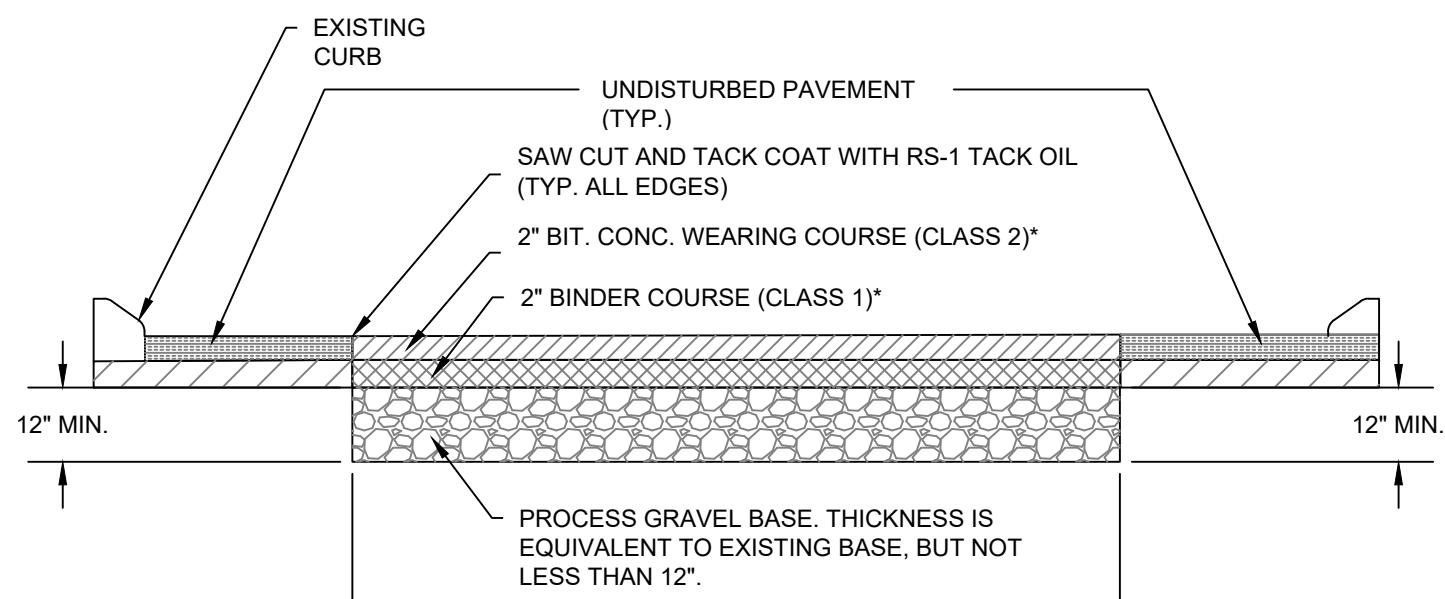
CONTRACT
DRAWING
**WATER MAIN, PAVEMENT AND EROSION
AND SEDIMENTATION CONTROL**
DETAILS

DESIGNED JFD	DRAWN KPP	DRAWN AS SHOWN DRS	CHECKED DRA	REVISED
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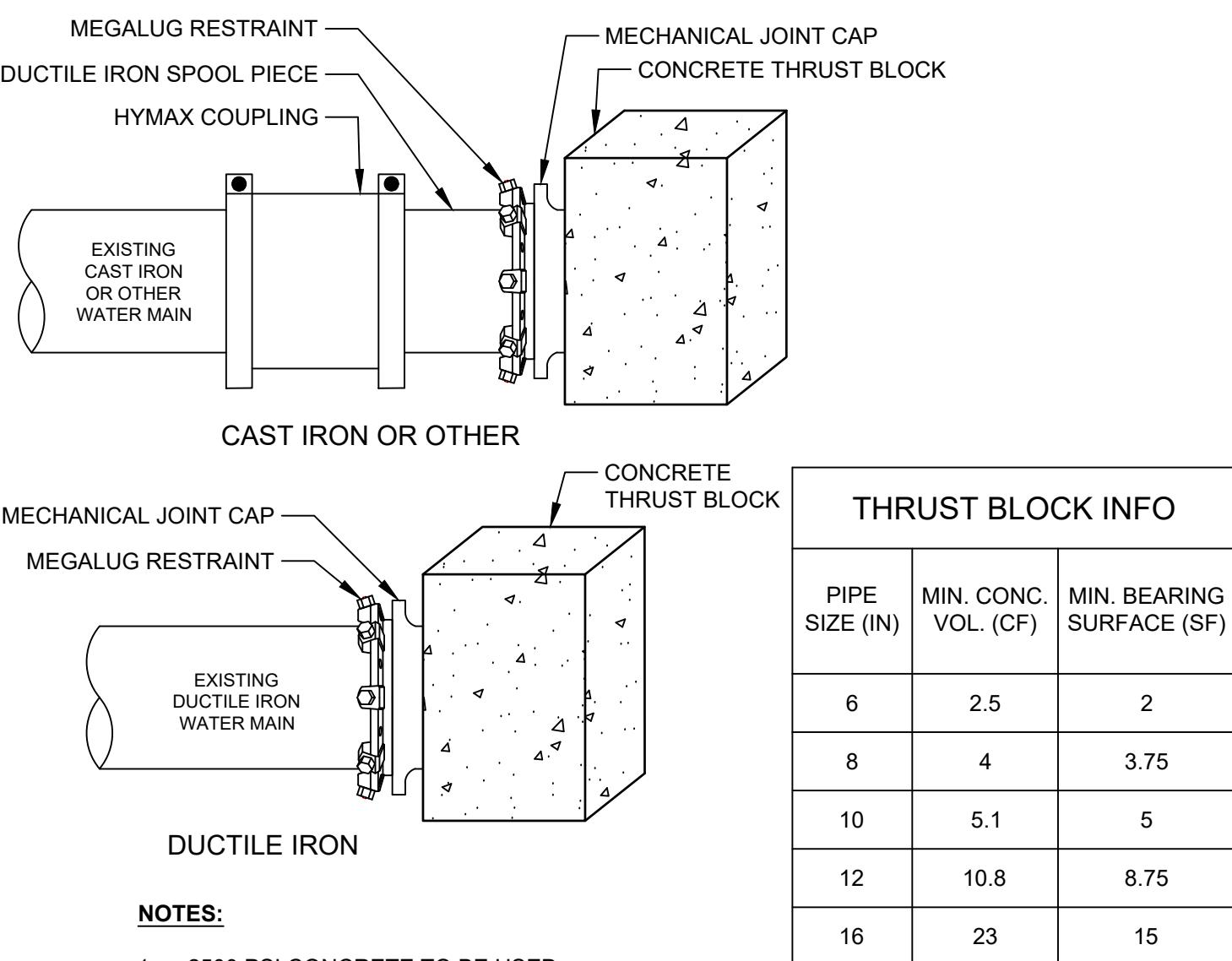
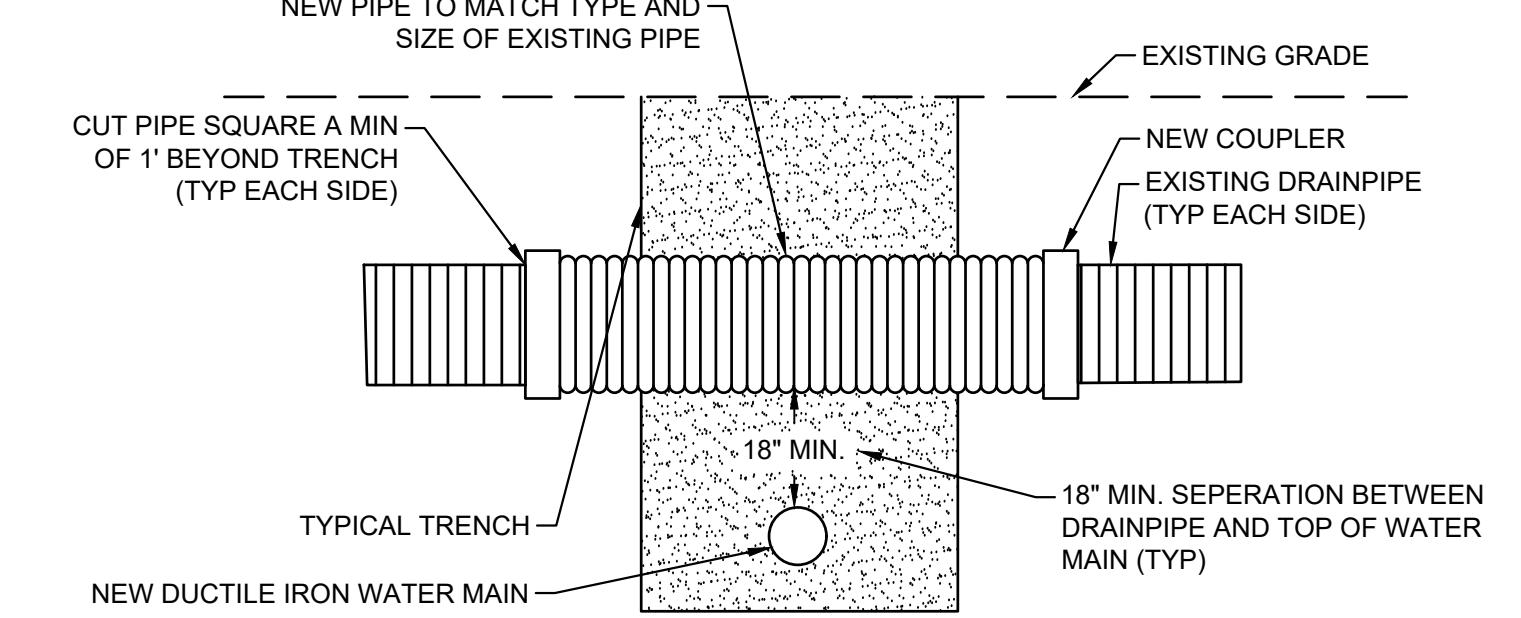
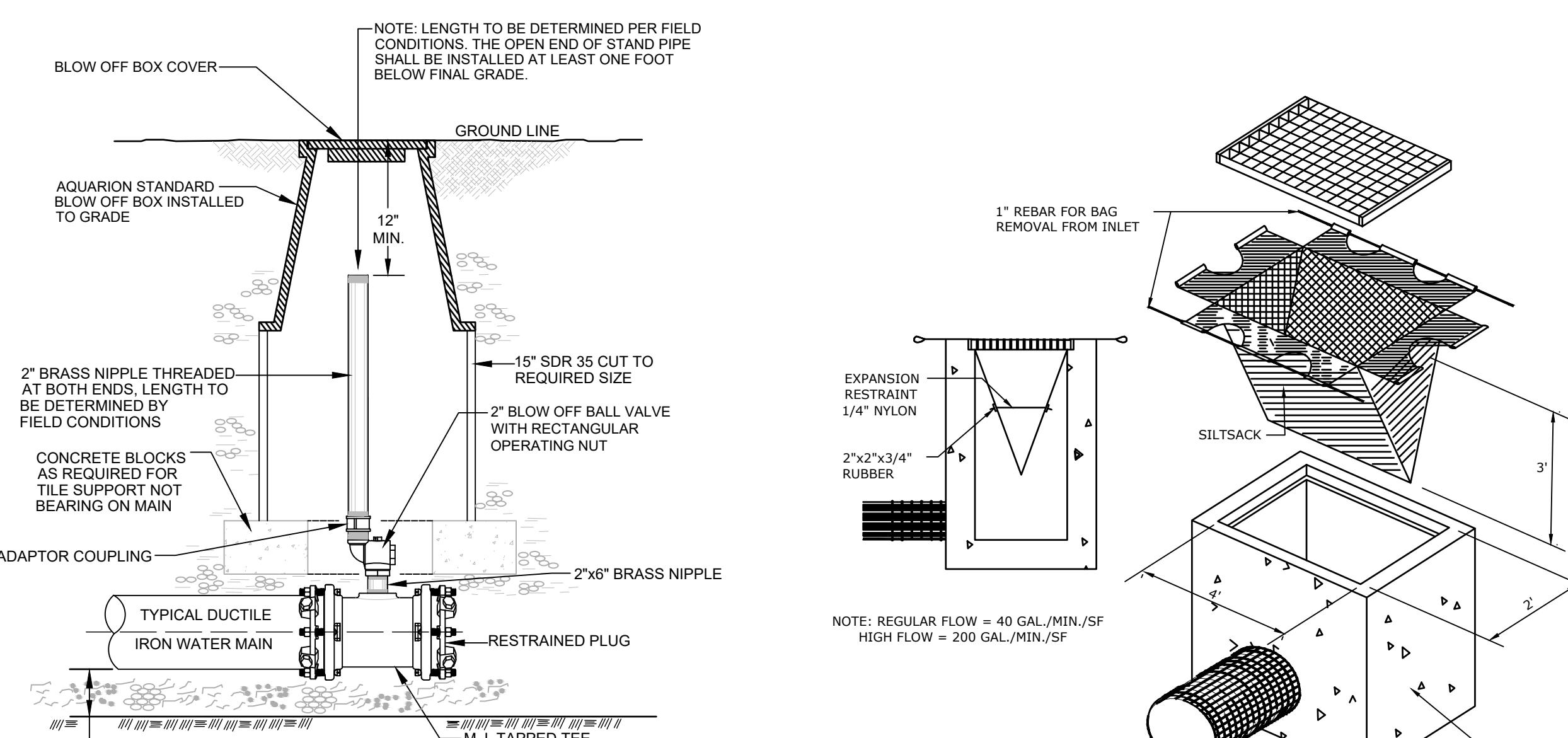
 PREPARED FOR:
TOWN OF
RIDGEFIELD
Ridgefield, Connecticut

**RIDGEFIELD HIGH SCHOOL
WATER MAIN EXTENSION**

JOB NO. AWC-221.02				
SHEET NO. 2 OF 6				CONTRACT DRAWING
JUN E 2023				


GATE VALVE DETAIL (AWC SD-2)
NOT TO SCALE

TYPICAL TRENCH DETAIL (AWC SD-1)
NOT TO SCALE

HYDRANT ASSEMBLY DETAIL (AWC SD-8)
NOT TO SCALE


*THE AQUARIAN WATER COMPANY RESERVES THE RIGHT TO ADD, SUBTRACT, OR MODIFY THE FINAL PAVEMENT RESTORATION LIMITS AND REQUIREMENTS AT NO ADDITIONAL COST TO THE AQUARIAN WATER COMPANY.

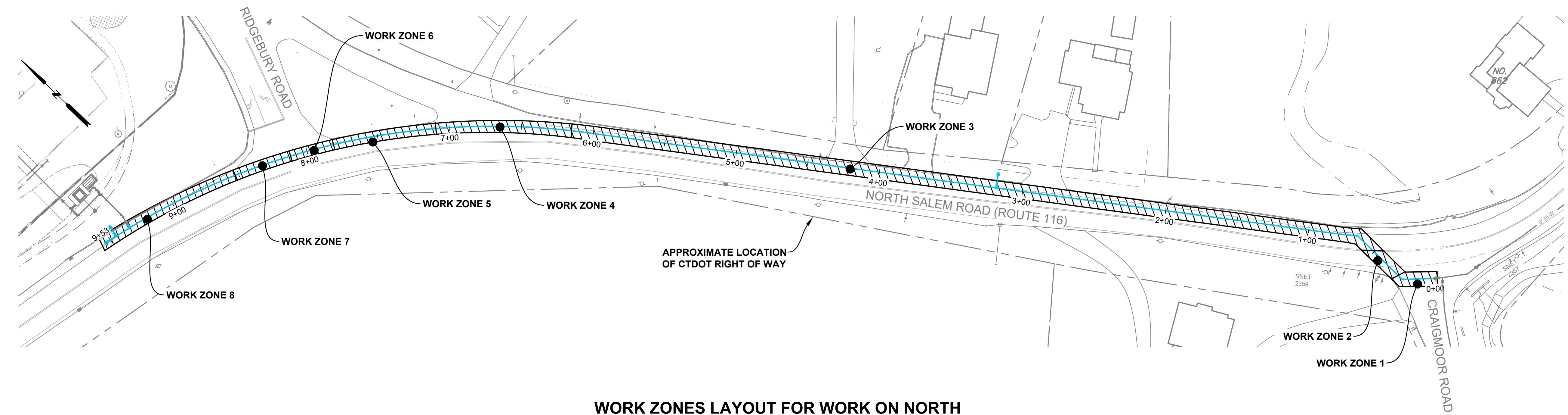
TYPICAL PAVEMENT DETAIL - TOWN ROAD
NOT TO SCALE

TYPICAL CUT AND CAP DETAIL
NOT TO SCALE

REPAIR OF UNKNOWN STORM DRAIN
NOT TO SCALE

BLOW-OFF AT DEAD END DETAIL (AWC SD-6)
NOT TO SCALE

SILT SACK DETAIL (AWC SD-26)
NOT TO SCALE

POLYETHYLENE ENCASED DUCTILE IRON PIPE RESTRAINT TABLE FOR HORIZONTAL BENDS AND DEAD ENDS				
LARGER PIPE SIZE	11-1/4° HORZ. BEND	22-1/2° HORZ. BEND	45° HORZ. BEND	90° HORZ. BEND DEAD END
6"	5 FT	5 FT	10 FT	25 FT 95 FT
8"	5 FT	10 FT	15 FT	35 FT 120 FT
10"	5 FT	10 FT	20 FT	40 FT 145 FT
12"	5 FT	10 FT	20 FT	45 FT 170 FT
16"	10 FT	15 FT	25 FT	60 FT 220 FT

POLYETHYLENE ENCASED DUCTILE IRON PIPE RESTRAINT TABLE FOR HORIZONTAL BENDS AND DEAD ENDS				
LARGER PIPE SIZE	11-1/4° HORZ. BEND	22-1/2° HORZ. BEND	45° HORZ. BEND	90° HORZ. BEND DEAD END
6"	5 FT	5 FT	10 FT	25 FT 95 FT
8"	5 FT	10 FT	15 FT	35 FT 120 FT
10"	5 FT	10 FT	20 FT	40 FT 145 FT
12"	5 FT	10 FT	20 FT	45 FT 170 FT
16"	10 FT	15 FT	25 FT	60 FT 220 FT

NOTE: RESTRAINT LENGTHS CALCULATED BASED ON A TEST PRESSURE OF 130 psi. FOR PRESSURES GREATER THAN 150 psi THE CONTRACTOR SHALL CONSULT THE ENGINEER.



WORK ZONES LAYOUT FOR WORK ON NORTH SALEM ROAD (RTE 116)

SCALE: 1" = 40'

TRAFFIC PLAN TABLE	
TRAFFIC PLAN TYPE	WORK ZONE NO.'S USING PLAN
TYPICAL TRAFFIC PLAN FOR WORK AT THE SIDE OF AN INTERSECTION	1,4,6,7
CLOSURE AT THE SIDE OF AN INTERSECTION	TO BE USED IF TWO 11' LANES CANNOT BE MAINTAINED ON "TYPICAL TRAFFIC PLAN FOR WORK AT THE SIDE OF AN INTERSECTION"
TYPICAL TRAFFIC PLAN FOR WORK IN THE CENTER OF AN INTERSECTION (PLAN 16)	2
CLOSURE IN THE CENTER OF AN INTERSECTION	TO BE USED IF TWO 11' LANES CANNOT BE MAINTAINED ON "TYPICAL TRAFFIC PLAN FOR WORK IN THE CENTER OF AN INTERSECTION"
WORK IN TRAVEL LANE AND SHOULDER TWO LANE HIGHWAY ALTERNATING ONE-WAY TRAFFIC OPERATIONS (PLAN 13)	3,5

WORK ZONES LAYOUT, TRAFFIC PLAN STA. 0+00 TO STA. 9+53

PREPARED FOR:
TOWN OF

RIDGEFIELD HIGH SCHOOL WATER MAIN EXTENSION

J U N E 2 0 2 3

SHEET NO. 3



PORTLAND, CONNECTICUT

CONTRACT DRAWING

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SHEET NO. 3

CONTRACT DRAWING

MANAGEMENT AND PROTECTION OF TRAFFIC PLAN NOTES AND DETAILS

PREPARED FOR:
TOWN OF
RIDGEFIELD
Ridgefield, Connecticut

RIDGEFIELD HIGH SCHOOL WATER MAIN EXTENSION

J U N E 2 0 2 3
S H E E T N O . 4 O F 6
J O B N O . A W C - 2 2 1 . 0 2

LOSURE AT THE SIDE OF AN INTERSECTION

NOT TO SCALE

Notes for Figure 6H-27—Typical Application 27 Closure at the Side of an Intersection

Guidance:

- The situation depicted can be simplified by closing one or more of the intersection approaches. If this cannot be done, and/or when capacity is a problem, through vehicular traffic should be directed to other roads or streets.
- Depending on road user conditions, flagger(s) or uniformed law enforcement officer(s) should be used to direct road users within the intersection.

Standard:

- At night, flagger stations shall be illuminated, except in emergencies.**

Option:

- Flashing warning lights and/or flags may be used to call attention to the advance warning signs.
- For short-duration work operations, the channelizing devices may be eliminated if a vehicle displaying high-intensity rotating, flashing, oscillating, or strobe lights is positioned in the work space.
- A BE PREPARED TO STOP sign may be added to the sign series.

Guidance:

- When used, the BE PREPARED TO STOP sign should be located before the Flagger symbol sign.
- ONE LANE ROAD AHEAD signs should also be used to provide adequate advance warning.

Support:

- Turns can be prohibited as required by vehicular traffic conditions. Unless the streets are wide, it might be physically impossible to make certain turns, especially for large vehicles.

Option:

- Vehicle hazard warning signals may be used to supplement high-intensity rotating, flashing, oscillating, or strobe lights.

Standard:

- Vehicle hazard warning signals shall not be used instead of the vehicle's high-intensity rotating, flashing, oscillating, or strobe lights.**

NOTES FOR TRAFFIC CONTROL PLANS

1. IF A TRAFFIC STOPPAGE OCCURS IN ADVANCE OF SIGN (A), THEN AN ADDITIONAL SIGN (A) SHALL BE INSTALLED IN ADVANCE OF THE STOPPAGE.
2. SIGNS (AA), (A), AND (D) SHOULD BE OMITTED WHEN THESE SIGNS HAVE ALREADY BEEN INSTALLED TO DESIGNATE A LARGER WORK ZONE THAN THE WORK ZONE THAT IS ENCOMPASSED ON THIS PLAN.
3. SEE TABLE 1 FOR ADJUSTMENT OF TAPERS IF NECESSARY.
4. IF THIS PLAN REMAINS IN CONTINUOUS OPERATION FOR MORE THAN 36 HOURS, THEN TRAFFIC DRUMS SHALL BE USED IN PLACE OF TRAFFIC CONES.
5. ANY LEGAL SPEED LIMIT SIGNS WITHIN THE LIMITS OF A ROADWAY / LANE CLOSURE AREA SHALL BE COVERED WITH AN OPAQUE MATERIAL WHILE THE CLOSURE IS IN EFFECT, AND UNCOVERED WHEN THE ROADWAY / LANE CLOSURE IS RE-OPENED TO ALL LANES OF TRAFFIC.
6. IF THIS PLAN REMAINS IN CONTINUOUS OPERATION FOR MORE THAN 36 HOURS, THEN ANY EXISTING CONFLICTING PAVEMENT MARKINGS SHALL BE ERADICATED OR COVERED, AND TEMPORARY PAVEMENT MARKINGS THAT DELINEATE THE PROPER TRAVELPATHS SHALL BE INSTALLED.
7. DISTANCES BETWEEN SIGNS IN THE ADVANCE WARNING AREA MAY BE REDUCED TO 100' ON LOW-SPEED URBAN ROADS (SPEED LIMIT < 40 MPH).
8. IF THIS PLAN IS TO REMAIN IN OPERATION DURING THE HOURS OF DARKNESS, INSTALL BARRICADE WARNING LIGHTS - HIGH INTENSITY ON ALL POST-MOUNTED DIAMOND SIGNS IN THE ADVANCE WARNING AREA.
9. A CHANGEABLE MESSAGE SIGN SHALL BE INSTALLED ONE HALF TO ONE MILE IN ADVANCE OF THE LANE CLOSURE TAPER.
10. SIGN (P) SHALL BE MOUNTED A MINIMUM OF 7 FEET FROM THE PAVEMENT SURFACE TO THE BOTTOM OF THE SIGN.

TABLE 1 - MINIMUM TAPER LENGTHS

POSTED SPEED LIMIT (MILES PER HOUR)	MINIMUM TAPER LENGTH FOR A SINGLE LANE CLOSURE
30 OR LESS	180' (55m)
35	250' (75m)
40	320' (100m)
45	540' (165m)
50	600' (180m)
55	660' (200m)
65	780' (240m)

METRIC CONVERSION CHART (1" = 25mm)

ENGLISH	METRIC	ENGLISH	METRIC	ENGLISH	METRIC
12"	300mm	42"	1050mm	72"	1800mm
18"	450mm	48"	1200mm	78"	1950mm
24"	600mm	54"	1350mm	84"	2100mm
30"	750mm	60"	1500mm	90"	2250mm
36"	900mm	66"	1650mm	96"	2400mm

SCALE: NONE

CONSTRUCTION TRAFFIC CONTROL PLAN

NOTES

CONNECTICUT DEPARTMENT OF TRANSPORTATION

BUREAU OF ENGINEERING & CONSTRUCTION

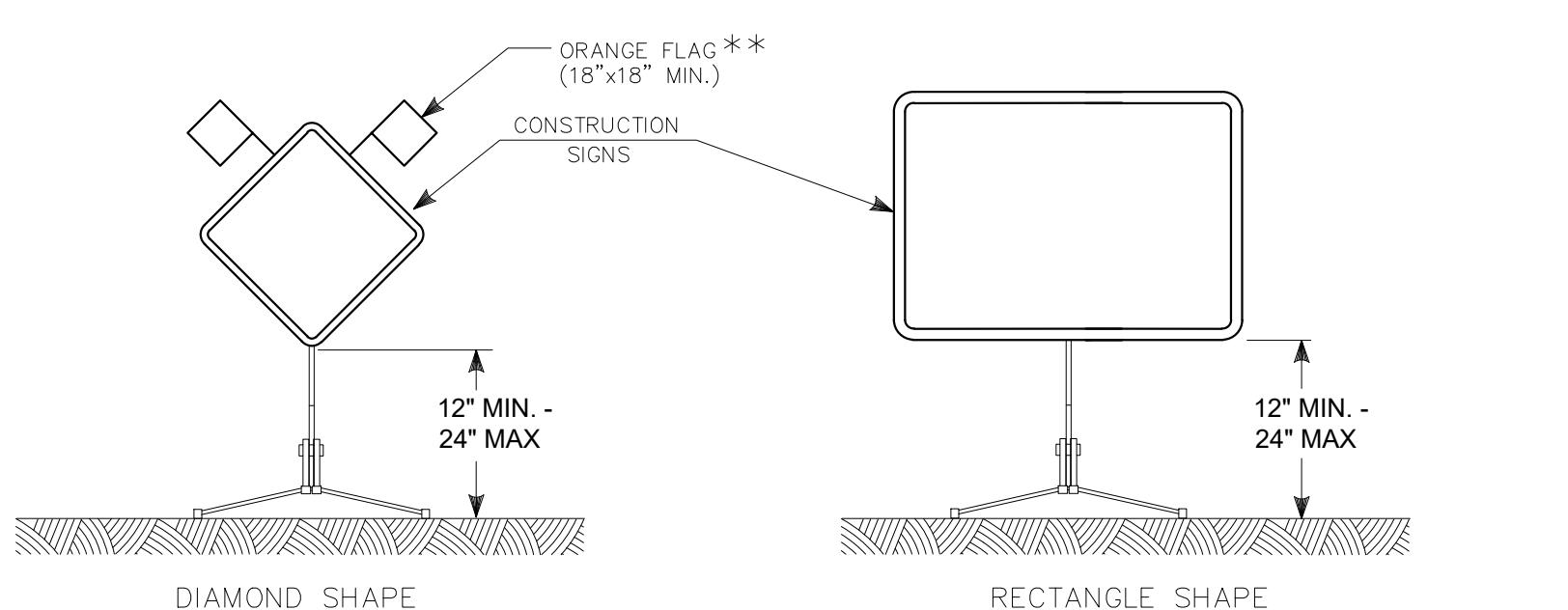
APPROVED

Charles S. Harlow
2012.06.05 15:50:35-04'00'

<p style="text-align: center;">WORK IN TRAVEL LANE AND SHOULDER TWO LANE HIGHWAY ALTERNATING ONE-WAY TRAFFIC OPERATIONS</p>	<p style="text-align: right;">SIGN FACE 108 SQ. FT (MIN.)</p>
<u>HAND SIGNAL METHODS TO BE USED BY UNIFORMED FLAGGERS</u>	
<p>THE FOLLOWING METHODS FROM SECTION 6E.07, FLAGGER PROCEDURES, IN THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES," SHALL BE USED BY UNIFORMED FLAGGERS WHEN DIRECTING TRAFFIC THROUGH A WORK AREA. THE STOP/SLOW SIGN PADDLE (SIGN NO. 80-9950) SHOWN ON THE TRAFFIC STANDARD SHEET TR-1220 01 ENTITLED, "SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS" SHALL BE USED.</p>	
<p>A. TO STOP TRAFFIC</p> <p>TO STOP ROAD USERS, THE FLAGGER SHALL FACE ROAD USERS AND AIM THE STOP PADDLE FACE TOWARD ROAD USERS IN A STATIONARY POSITION WITH THE ARM EXTENDED HORIZONTALLY AWAY FROM THE BODY. THE FREE ARM SHALL BE HELD WITH THE PALM OF THE HAND ABOVE SHOULDER LEVEL TOWARD APPROACHING TRAFFIC.</p>	
<p>B. TO DIRECT TRAFFIC TO PROCEED</p> <p>TO DIRECT STOPPED ROAD USERS TO PROCEED, THE FLAGGER SHALL FACE ROAD USERS WITH THE SLOW PADDLE FACE AIMED TOWARD ROAD USERS IN A STATIONARY POSITION WITH THE ARM EXTENDED HORIZONTALLY AWAY FROM THE BODY. THE FLAGGER SHALL MOTION WITH THE FREE HAND FOR ROAD USERS TO PROCEED.</p>	
<p>C. TO ALERT OR SLOW TRAFFIC</p> <p>TO ALERT OR SLOW TRAFFIC, THE FLAGGER SHALL FACE ROAD USERS WITH THE SLOW PADDLE FACE AIMED TOWARD ROAD USERS IN A STATIONARY POSITION WITH THE ARM EXTENDED HORIZONTALLY AWAY FROM THE BODY. TO FURTHER ALERT OR SLOW TRAFFIC, THE FLAGGER HOLDING THE SLOW PADDLE FACE TOWARD ROAD USERS MAY MOTION UP AND DOWN WITH THE FREE HAND, PALM DOWN.</p>	
<p><input type="checkbox"/> TRAFFIC CONE OR TRAFFIC DRUM <input checked="" type="checkbox"/> OPTIONAL <input type="checkbox"/> TRAFFIC DRUM <input type="checkbox"/> PORTABLE SIGN SUPPORT <input type="checkbox"/> HIGH MOUNTED INTERNALLY ILLUMINATED FLASHING ARROW</p>	<p style="text-align: right;">CONSTRUCTION TRAFFIC CONTROL PLAN PLAN 13 - SHEET 2 OF 2 SEE NOTES 1, 2, 4, 6, 7, 8</p>

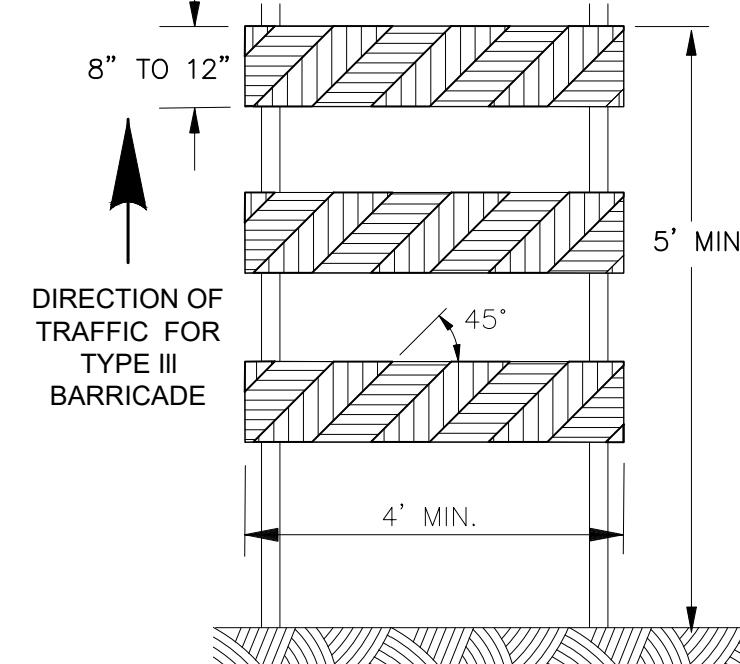
TRAFFIC CONTROL NOTES:

1. THE CONTRACTOR SHALL PROVIDE ACCESS FOR EMERGENCY VEHICLES AT ALL TIMES.
2. CONTRACTOR TO BAG OR REMOVE ALL SIGNS WHEN THEY ARE NOT NEEDED.
3. THE CONTRACTOR SHALL MAINTAIN ACCESS TO PROPERTIES.
4. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN CROSSINGS AT INTERSECTIONS.
5. THE CONTRACTOR WILL BE RESPONSIBLE FOR OBTAINING PERMITS FROM THE MUNICIPALITY AND/OR CONNECTICUT DEPARTMENT OF TRANSPORTATION (CTDOT).
6. UNIFORMED POLICE OFFICERS MAY BE REQUIRED FOR TRAFFIC CONTROL.
7. TEMPORARY TRAFFIC CONTROL ON STATE ROADWAYS SHALL CONFORM TO CTDOT CONSTRUCTION TRAFFIC CONTROL PLANS.
8. CONSTRUCTION SIGNS SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING AND SHALL CONFORM TO SECTION 12.20 OF THE CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATION FORM 818. SIGNS MAY BE MOUNTED ON EITHER METAL SIGN POSTS OR PORTABLE SIGN SUPPORTS.
9. THE CONTRACTOR SHALL RELOCATE AND/OR REMOVE SIGNS, PAVEMENT MARKINGS, BARRIERS, DELINEATORS, ETC. AS THE WORK PROGRESSES.
10. THE CONTRACTOR SHALL COVER ALL CONFLICTING PAVEMENT MARKINGS WITH PREFORMED BLACK LINE MASK PAVEMENT MARKING TAPE AND SHALL REMOVE WHEN NO LONGER APPLICABLE (UNLESS OTHERWISE SPECIFIED WITHIN THE CONTRACT DOCUMENTS).
11. UPON COMPLETION OF THE SURFACE PAVEMENT RESTORATION, THE CONTRACTOR SHALL FURNISH AND INSTALL PERMANENT PAVEMENT MARKINGS (I.E. CENTERLINES, LANE LINES, STOP BARS, CROSSWALKS, BUS/BICYCLE LANE MARKINGS, ETC.) INTERIM TEMPORARY MARKINGS MAY BE REQUIRED.
12. LOCATIONS FOR CONSTRUCTION SIGNS ARE APPROXIMATE AND SHALL BE ADJUSTED AS DIRECTED BY THE OWNER AND PLACED APPROPRIATELY FOR FIELD CONDITIONS.
13. CONE SPACING AND TAPER LENGTHS MAY NEED TO BE ADJUSTED TO MEET FIELD CONDITIONS.
14. TRAFFIC CONES SHALL BE 42 INCHES IN HEIGHT.
15. EXISTING SIGNS IN CONFLICT WITH TEMPORARY SIGNS SHALL BE COVERED.
16. CONTRACTOR SHALL MAINTAIN SIDEWALK ACCESS ON AT LEAST ONE SIDE OF THE STREET.
17. CONTRACTOR TO MAINTAIN ACCESS FOR ANY SCHOOL BUS OR PUBLIC BUS AND COORDINATE WITH LOCAL PROVIDERS, AS NECESSARY.
18. ALL COMMERCIAL AND PRIVATE DRIVES MUST REMAIN OPEN, IF DURING CONSTRUCTION, ACCESS TO THE AFFECTED BUSINESS IS NOT APPARENT, ADDITIONAL SIGNS (I.E. BUSINESS ACCESS) MAY BE REQUIRED.
19. TRAFFIC CONTROL PERSONS TO HAVE WALKIE TALKIES WHEN WORK ZONE IS IN A CURVE OR THERE ARE SIGHT DISTANCE RESTRICTIONS.
20. THE CONTRACTOR SHALL MAINTAIN TRAFFIC CONTROL MEASURES AT ALL TIMES.
21. THE CONTRACTOR IS RESPONSIBLE TO PROVIDE AND MAINTAIN PEDESTRIAN WALKWAYS WHERE EXISTING WALKWAYS ARE IN CLOSE PROXIMITY TO EXCAVATIONS OR WORK AREAS OR ARE OTHERWISE OBSTRUCTED PER SECTION 01570 OF THE PROJECT MANUAL.
22. CONTRACTOR SHALL NOTIFY CONNECTICUT TRANSIT PRIOR TO IMPLEMENTING DETOURS. THE CONTRACTOR SHALL COORDINATE WITH CT TRANSIT TWO WEEKS IN ADVANCE OF IMPACTING STOP LOCATIONS.
23. CONTRACTOR SHALL INSTALL NEW TRAFFIC DETECTOR LOOPS WHERE EXISTING LOOPS ARE DISTURBED DURING CONSTRUCTION.
24. WORK ZONE SIZES ARE SHOWN AS APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION TO ENSURE ADEQUATE SPACE FOR REQUIRED EQUIPMENT.
25. THE CONTRACTOR SHALL RELOCATE AND/OR REMOVE SIGNS AND OTHER TRAFFIC CONTROL DEVICES AS THE WORK PROGRESSES.
26. CONTRACTOR SHALL NOTIFY THE MUNICIPALITY INCLUDING FIRE AND POLICE AT LEAST TWO WEEKS PRIOR TO A ROAD CLOSURE.



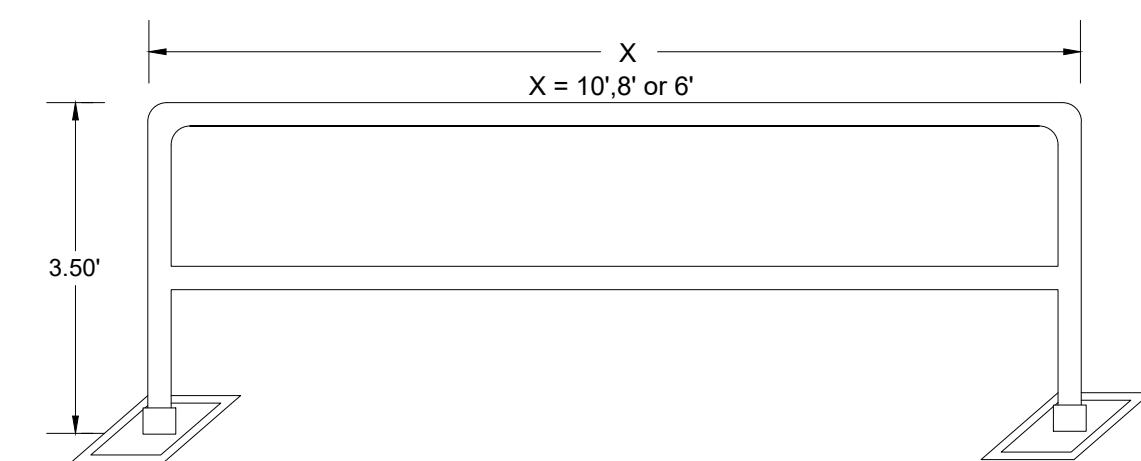
NOTES FOR PORTABLE SIGN SUPPORTS:

1. SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
2. MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12' AND A MAXIMUM OF 24'. SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE OWNER.
3. THE TOWN RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
4. PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3). SIGNS NEED TO BE STABILIZED AS NEEDED, WHICH MAY INCLUDE SAND BAGS OR OTHER WEIGHTED DEVICES.
5. ROLL-UP SIGNS ARE PERMITTED FOR SHORT DURATION WORK ZONES (LESS THAN 1 HOUR).
6. USE 48" SIGNS ON ARTERIAL ROADWAYS WHEN FEASIBLE.

CONSTRUCTION SIGNS
NOT TO SCALE

NOTES:

1. CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
2. MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" WIDE STRIPES SHALL BE USED.
3. THE TOWN RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
4. CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
5. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.

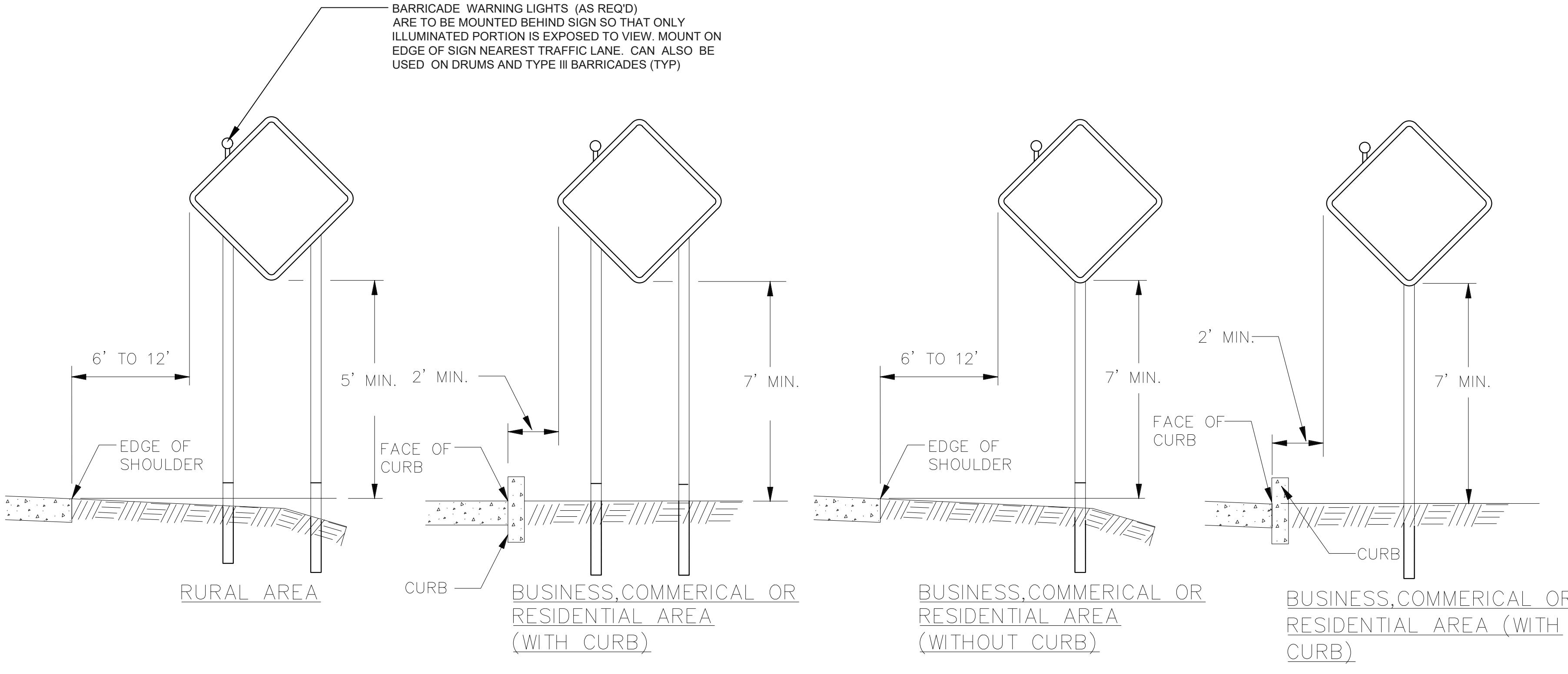
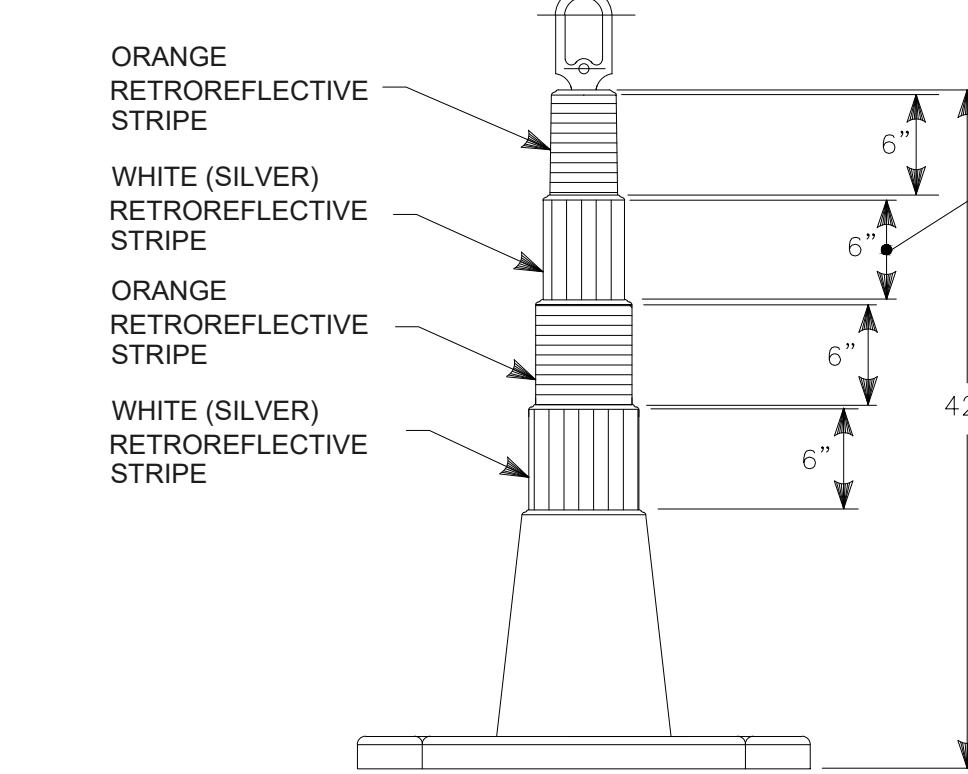
TYPE III BARRICADE
NOT TO SCALE

NOTES:

1. PEDESTRIAN GUIDE RAILS SHALL BE FREE STANDING, PORTABLE MEET ALL APPLICABLE OSHA AND ANSI STANDARDS.
2. PEDESTRIAN GUIDE RAILS SHALL BE POWDER COATED YELLOW.
3. PEDESTRIAN GUIDE RAIL BASE PLATES SHALL BE HOT DIPPED GALVANIZED FOR DURABILITY AND BE SKID RESISTANT,

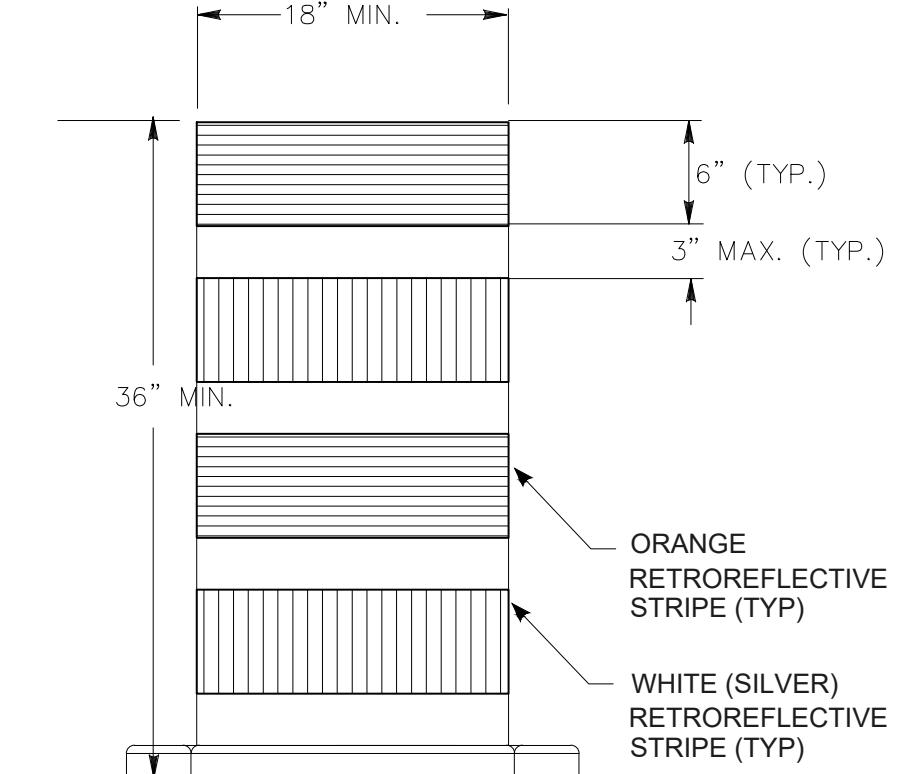
PEDESTRIAN GUIDE RAIL
NOT TO SCALECONSTRUCTION SIGNS, BARRICADES,
CONES AND DRUMS DETAILS

DESIGNED	DRAWN	SCALE AS SHOWN	REVIEWED
JFD	KPP	DRS	

PLACEMENT OF CONSTRUCTION SIGNS
TYPICAL LONG TERM INSTALLATION
NOT TO SCALE

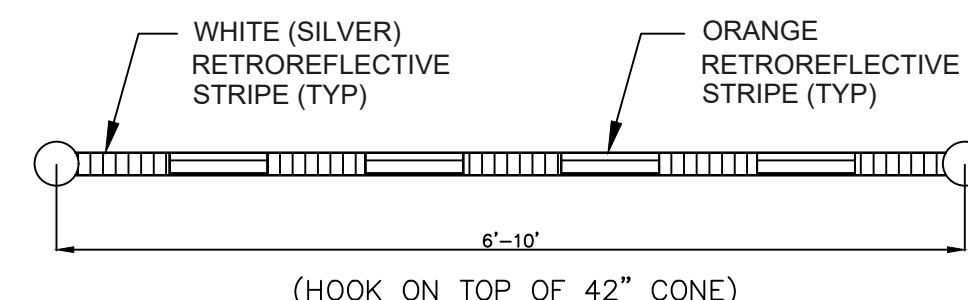
NOTES:

1. TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
2. IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
3. IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
4. THE TOWN RESERVES THE RIGHT TO REJECT ANY CONES DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.

42" TRAFFIC CONE
NOT TO SCALE

NOTES:

1. TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
2. THE TOWN RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
3. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
4. THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.

TRAFFIC DRUM
NOT TO SCALETRAFFIC CONE BAR
NOT TO SCALERIDGEFIELD HIGH SCHOOL
WATER MAIN EXTENSION
Ridgefield, Connecticut

PREPARED FOR:
TOWN OF
RIDGEFIELD
Ridgefield, Connecticut

DESIGNED	DRAWN	SCALE AS SHOWN	REVIEWED
JFD	KPP	DRS	

JUN E 2 0 2 3
SHEET NO. 5 OF 6
JOB NO. AWC-221.02
CONTRACT DRAWING
SHEET NO. 5
SHEET NO. 5 OF 6
JOB NO. AWC-221.02

